

WELCOME

Welcome to the Rock Island Shared Use Path Public Hearing. This is your opportunity to review and provide written or spoken comments on the preliminary design for the shared use path.

Available information includes:

- Layout and locations of the path
- Trailhead locations
- Construction limits
- Potential needs for additional right of way and easements
- Findings from the environmental review

Staff from the County and the design team are available to answer your questions and to provide additional information. We are here for you!

Thank you for attending tonight.

Your input is a valuable part of the environmental review process.

STAY CONNECTED!

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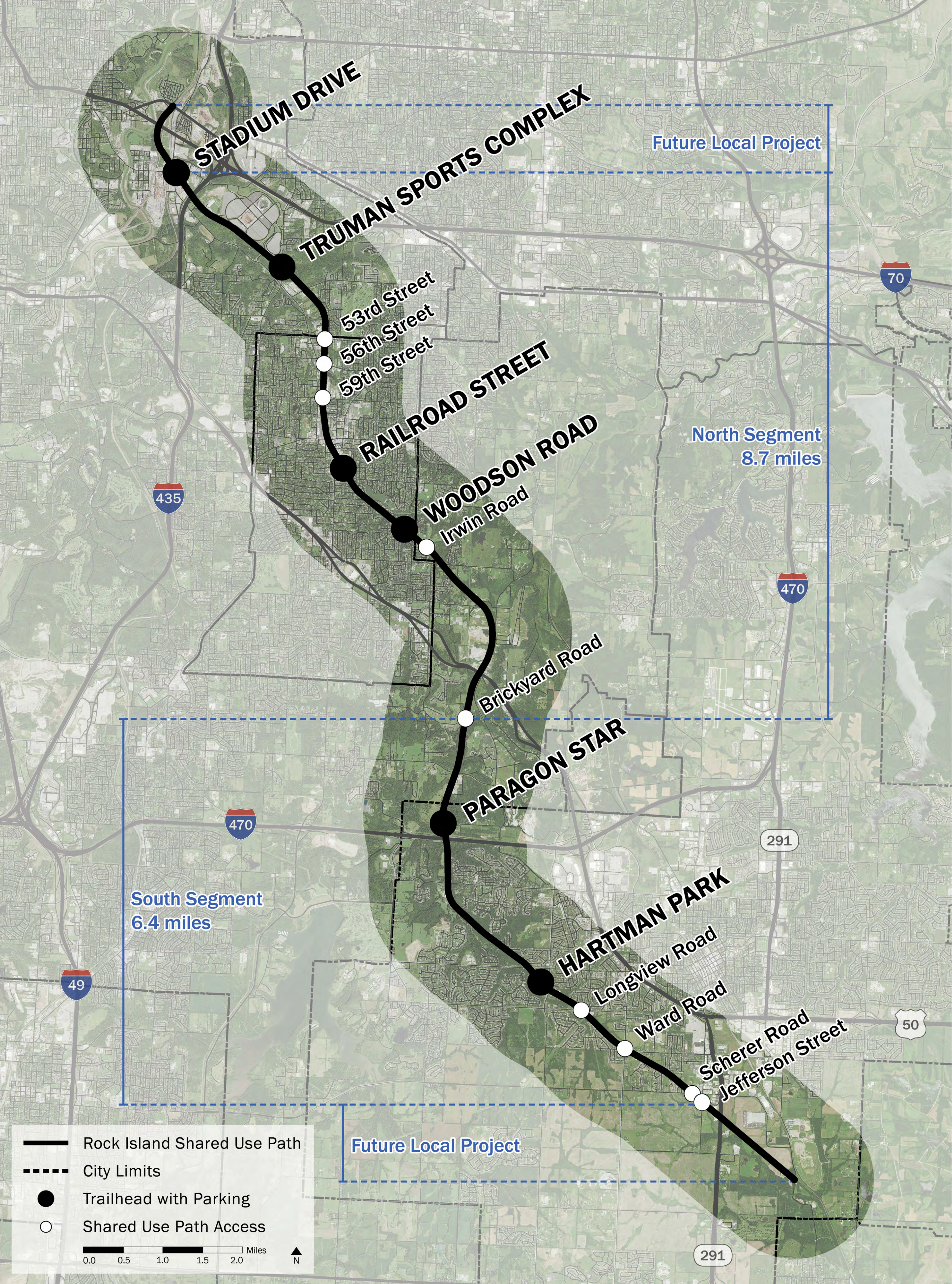
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CORRIDOR OVERVIEW



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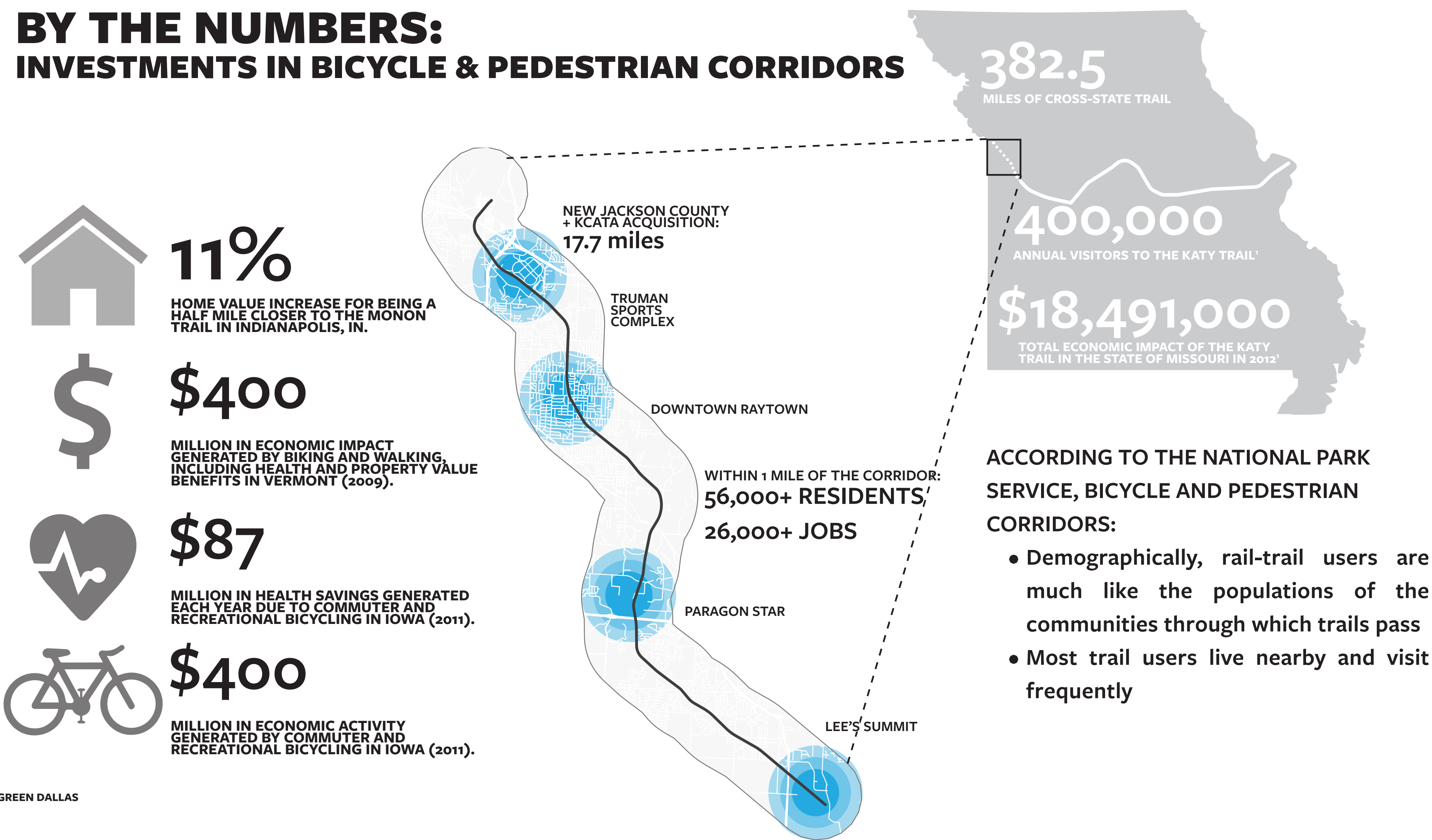


PROJECT OVERVIEW

PROJECT DESCRIPTION, PURPOSE AND NEED

The Rock Island Corridor is a **17.7-mile out-of-service freight railroad corridor** that spans three cities in the Greater Kansas City area – Kansas City, Raytown, and Lee’s Summit. The corridor **can simultaneously accommodate transit and bicycle and pedestrian facilities**. An approximately 15.6-mile long walking and biking path will eventually connect the Kansas City metro area with the cross-state Rock Island Trail State Park and the Katy Trail State Park. In the future, transit will help commuters beat long highway commutes with a comfortable option to get to work, run errands, or even go to a ball game.

BY THE NUMBERS: INVESTMENTS IN BICYCLE & PEDESTRIAN CORRIDORS



NEPA OVERVIEW

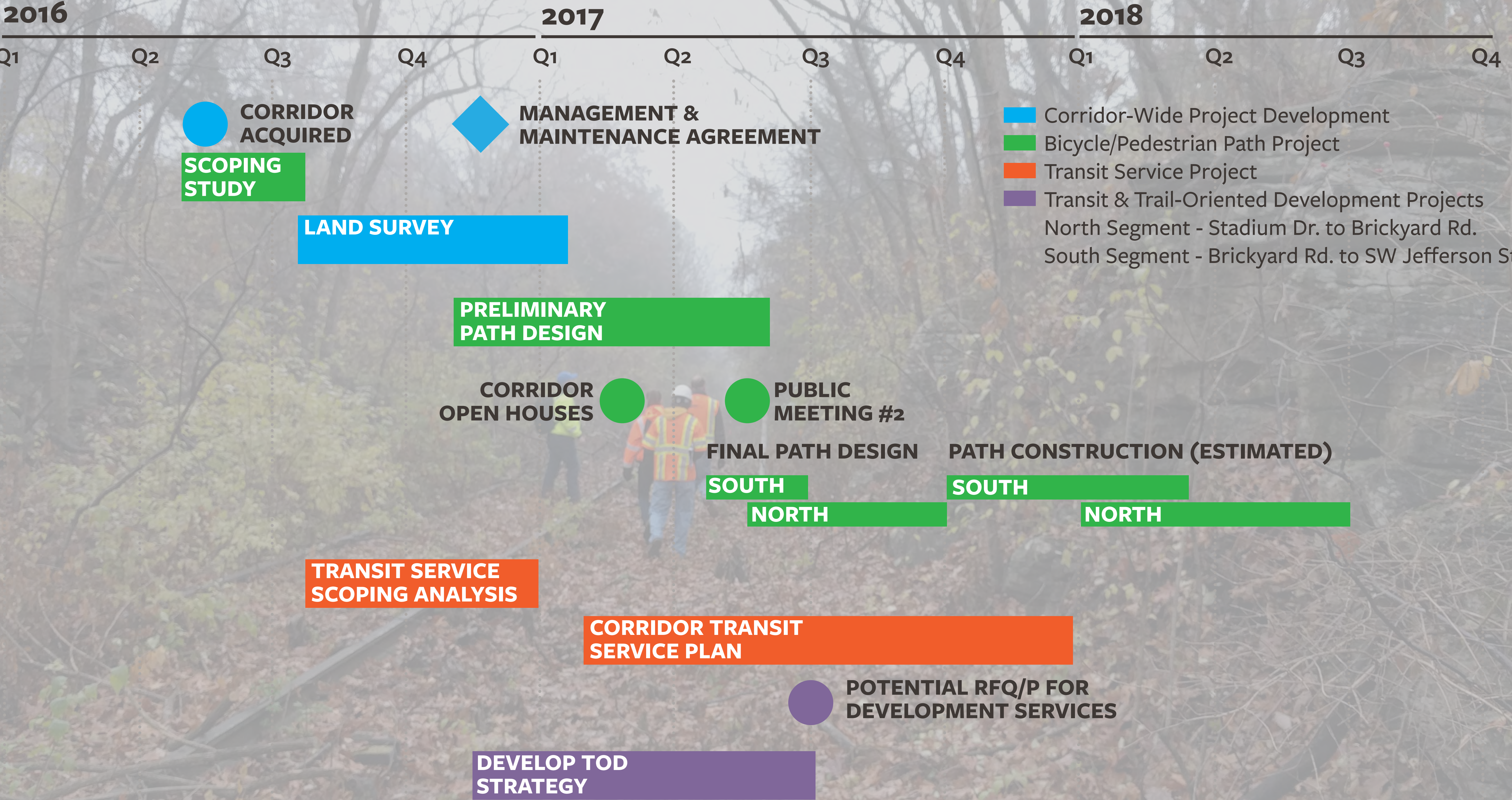
The National Environmental Policy Act (NEPA) established the requirements and processes that agencies must consider related to the environmental effects of any project that will use federal funding.

The Rock Island Rail Corridor Authority has done an environmental review of the project area, which includes:

- Identifying social, economic, and environmental impacts and any needed mitigation.
- Analyzing reasonable alternatives to the proposed project.
- Coordinating and consulting with other agencies.
- Providing opportunities for the public to comment.

Where We're Going

Anticipated Corridor Project Development Timeline



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ENVIRONMENTAL REVIEW

As a part of the environmental study, required as part of the National Environmental Policy Act (NEPA), the project team has surveyed the Rock Island corridor and looked at:

Community Impacts

While the path will be built on the original Rock Island right of way, some additional property will need to be acquired to construct the trailheads. Additional permanent and temporary easements will also be needed for drainage facilities and grading. Necessary property acquisitions will:

- Follow federal and state laws
- Require fair and equitable compensation when private property is purchased

Areas where right of way or easements need to be acquired are shown on the scroll maps and preliminary plans available at this meeting.



The project team is working to finalize right of way plans for the northern segment of the corridor.

Hazardous Waste

A detailed assessment was conducted that included field investigations and a records search to determine the likelihood of soil or groundwater contamination within the Rock Island corridor.

Based on the findings, the following will be done during construction:

- Waste materials found within the corridor, including ties and rail, will be disposed of following state regulations.
- Any additional soil samples will be coordinated with the Missouri Department of Natural Resources to determine appropriate course of action.
- Former rail bed soils, if removed from the site, will be transported and disposed of in compliance with regulatory procedures.



Any removal of former rail bed soils from the site will follow established regulatory procedures.

Water Quality Impacts

The path crosses or lies in the immediate vicinity of several creeks, rivers, and waterways. Construction permitting will include land disturbance permits and a stormwater pollution prevention plan. Other best management practices will also be used to protect and make sure that there are no adverse impacts to the water quality as a result of the project.



Implementation of a stormwater pollution prevention plan and other best management practices will help ensure the project has adverse impact to water quality.

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Threatened and Endangered Species

No state-listed endangered species, state-ranked species, and/or natural communities were identified within the project alignment, based on a records search.

Preferred roosting habitat for the Indiana and Northern Long-Eared bats, which are federally listed species, were identified at several locations along the corridor. Due to the presence of multiple potential summer roosting habitats:

- Removal of potential roost trees will be avoided to the extent possible
- Any required clearing of roost trees will be done during approved time periods

Floodplain Impacts

The path intersects 100-year floodplain and regulatory floodway zones at several locations. County and city requirements will be followed in FEMA regulated areas. The path will be concrete in floodplain and other low lying areas that are subject to potential overtopping.



The FEMA Flood Insurance Rate Map shows the flood risk zones near the Rock Island Corridor.



Due to the presence of potential summer roosting habitats, it is recommended that tree clearing should occur from November 1 to March 31 within the project corridor.

Wetland and Watercourse Impacts

Wetlands are a valuable natural resource which support filtration of stormwater runoff while providing habitat for important plant species. A detailed survey was conducted to assess the presence of potential areas requiring protection. Avoiding or minimizing impacts to those areas was considered as plans were developed for the path. A small amount of wetlands and channels will be impacted, and will require a permit from the United States Army Corps of Engineers prior to construction.



The wetland and watercourse review was conducted following methods set forth by the United States Army Corp of Engineers.

ENVIRONMENTAL REVIEW

As a part of the environmental study, required as part of the National Environmental Policy Act (NEPA), the project team has surveyed the Rock Island corridor and looked at:

Cultural and Historical Resources

No structures documented on the National Register of Historic Places exist within or adjacent to the corridor. The Missouri Department of Natural Resources State Historic Preservation Office (SHPO) has said that there is potential for above-ground historic properties, including the rail corridor. In response, a formal historic and architecture survey was conducted and the SHPO is currently reviewing those results. If cultural materials are encountered during project activities, all construction would stop and the SHPO would determine the appropriate course of action.

Parkland

Since the Rock Island Shared Use Path will be constructed partially with federal funds, the project must be assessed under Section 4(f) of the Department of Transportation Act as it relates to use of public parkland for transportation purposes.

The Rock Island corridor crosses an existing access road and trail serving Charles David Hartman Memorial Park near Pryor Road in Lee’s Summit. Vehicle access to the park will be maintained throughout construction with temporary bicycle/ pedestrian detours provided. The path passes within the general vicinity of several other public parks that lie outside the construction limits, but those parks will not be impacted by the path’s construction.



Temporary sidewalk detours will be provided during construction to maintain access to Charles David Hartman Memorial Park.



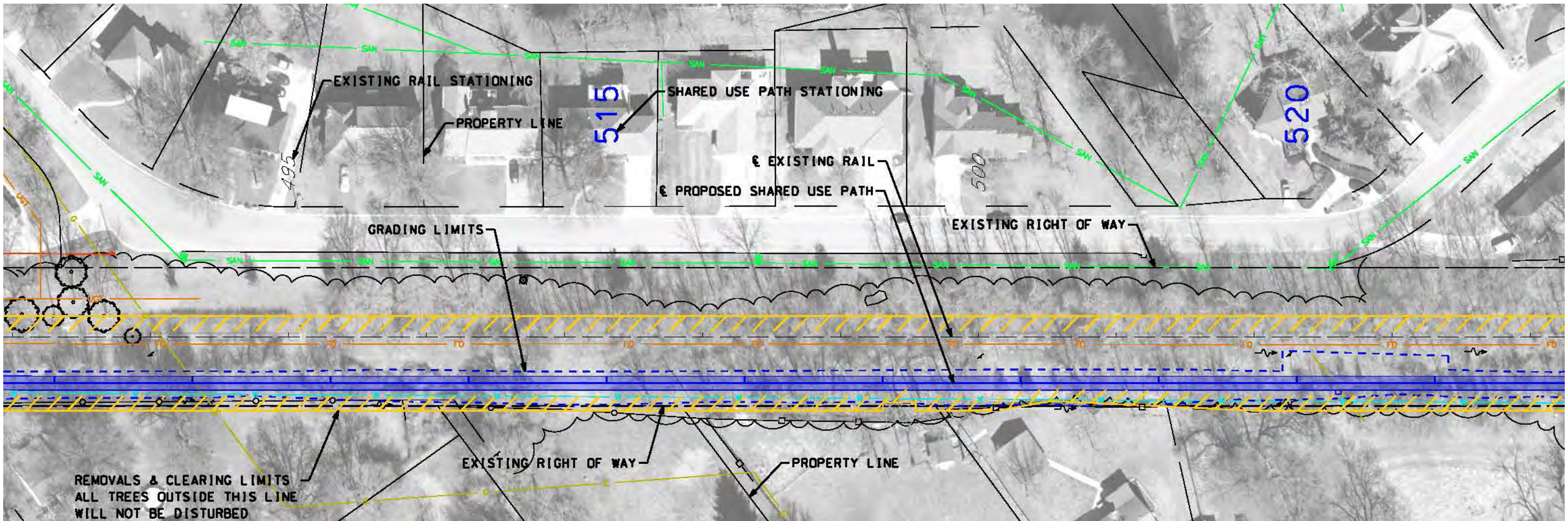
Fieldwork was conducted along the corridor for archaeological investigation in December 2016.

Other Resources Evaluated but Determined Not to be Impacted Include:

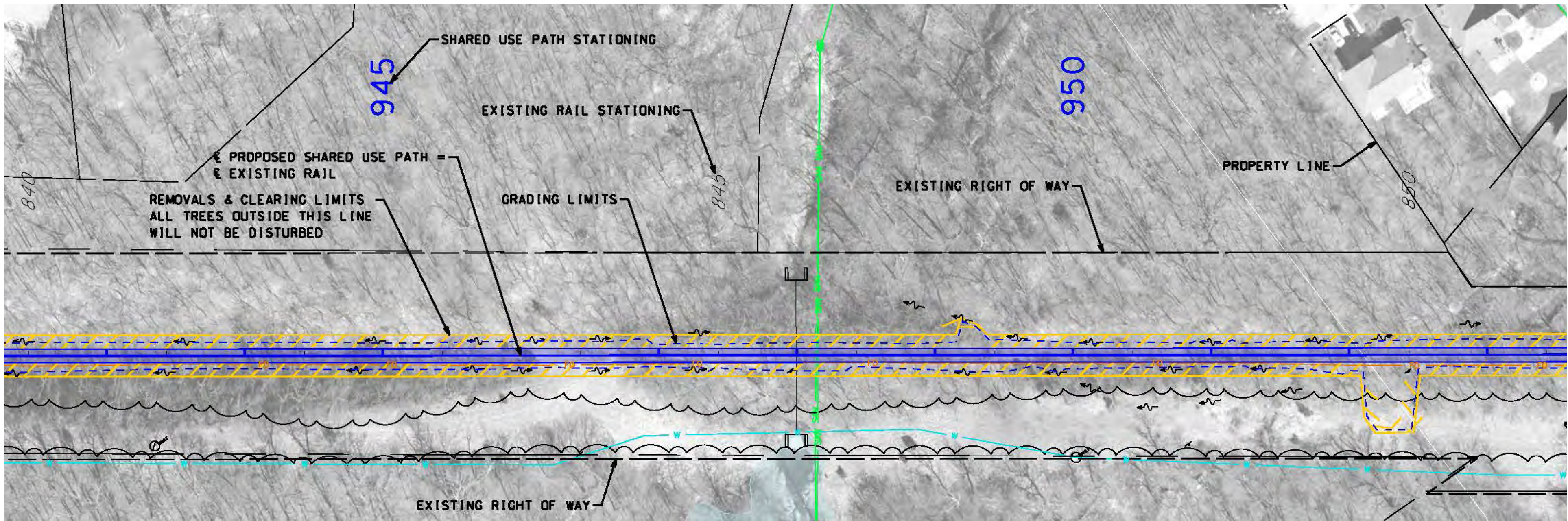
- Farmland
- Air Quality
- Noise




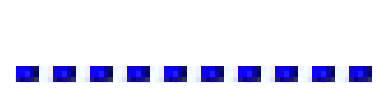



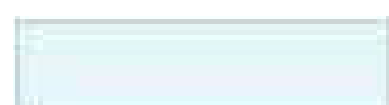
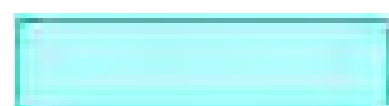

DESIGN PLANS LEGEND

Shared Use Path Offset Existing Rail Alignment



Shared Use Path on Existing Rail Alignment



-  Proposed Shared Use Path
-  Proposed Bridge
-  Proposed Retaining Wall
-  Proposed Construction Limits
-  Proposed Removals and Clearing Limits
-  Proposed Right of Way
-  Existing Right of Way
-  Existing Flood Plain
-  Existing Floodway
-  Existing Wetland Area

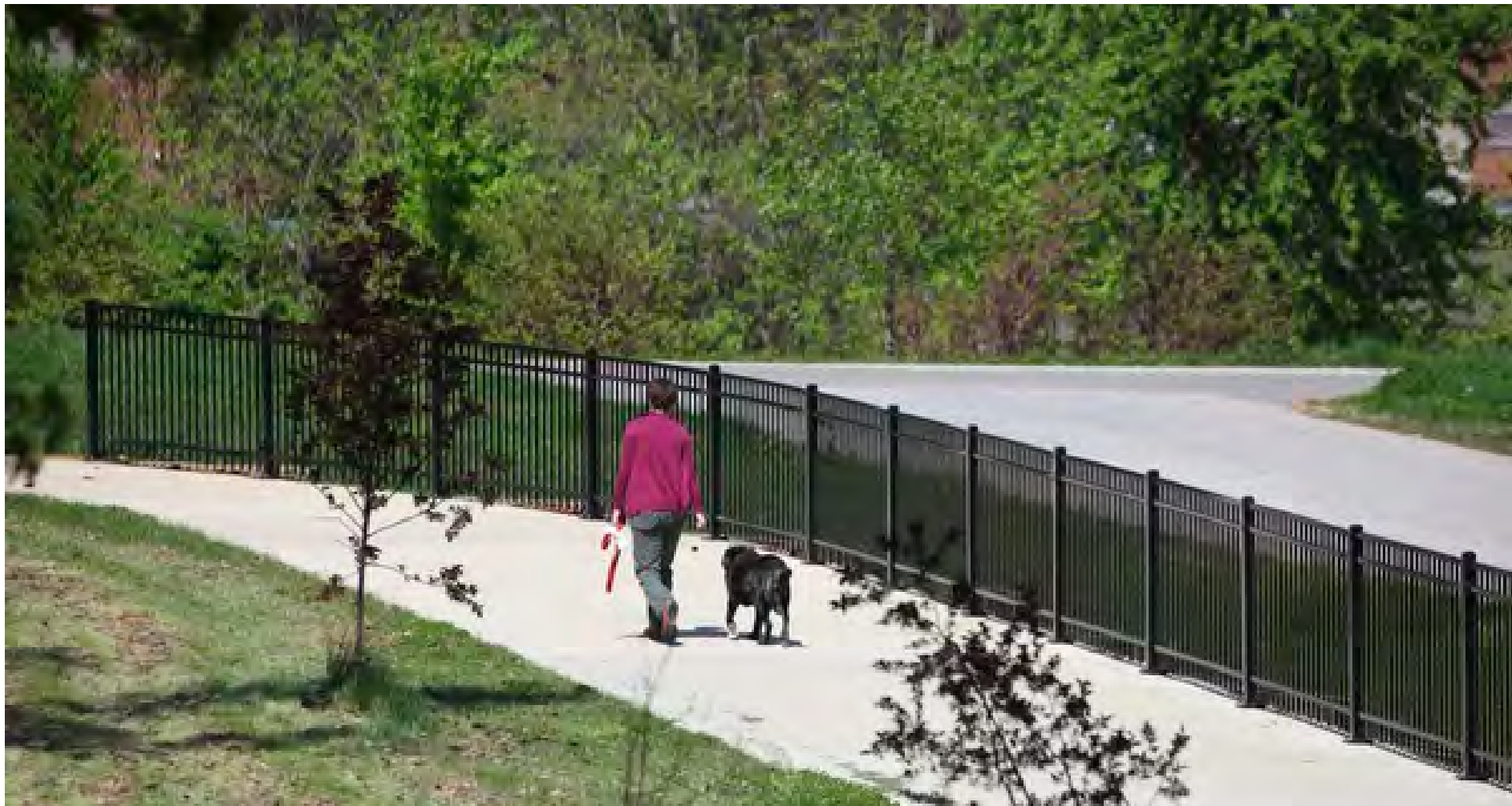
DESIGN CRITERIA

Fencing

Fencing will be provided at all bridges and at the edge of the path at high fills and other select locations. A black steel decorative fence will be installed on bridges and retaining walls. Woven wire fence, similar to that installed in rural and agricultural areas will be installed in other locations as deemed appropriate.



Example woven wire fence from a portion of the Rock Island Trail south of the corridor.



Example of the decorative steel fences to be installed on bridges in the corridor.

Street Crossings

Actuated flashing beacons will provide advanced warning at some locations where the path crosses the street.



Example of an advanced warning beacon to be installed at some street crossings.

Retaining Walls

Retaining walls will be required at numerous locations to build the trail in fill sections and cut sections. The wall type used will be a large block retaining wall system. Walls will be constructed at locations that:

- Conflict with utilities
- Conflict with existing fill slope
- To keep grading limits within right of way



Example of a large block retaining wall system.

Bannister Road Tunnel

The existing Bannister Road tunnel will be used to carry the trail under Bannister Road and will have LED lighting installed. Should future transit or rail operations return to the corridor, this portion of the trail will be relocated to maintain continuity.



The existing Bannister Road tunnel.



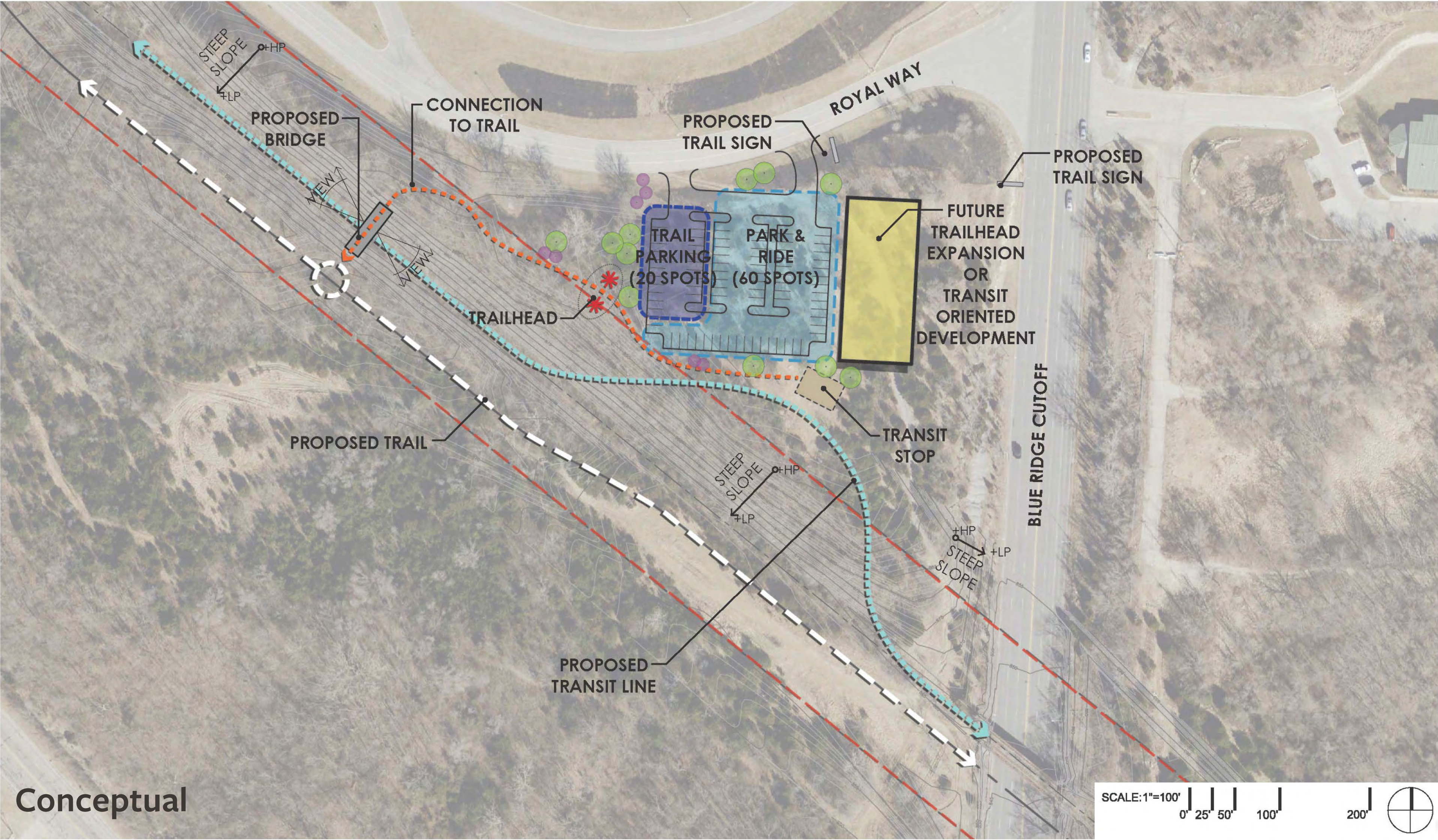
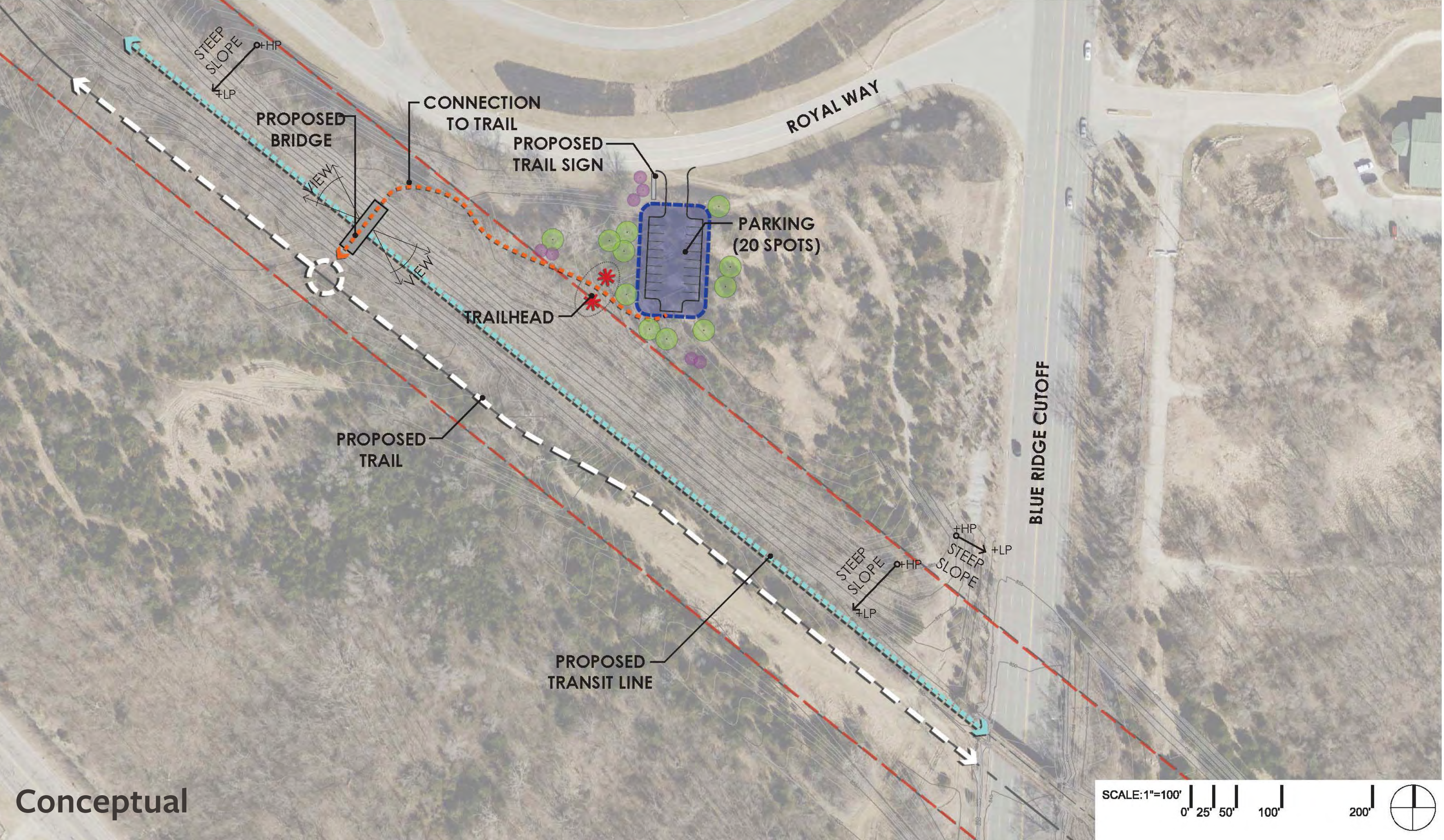
Example rendering of LED lighting to be installed in the Bannister Road tunnel.

STADIUM DRIVE TRAILHEAD PLANNING



Trailheads are the primary points where the path will be accessed from the public street system and other transportation modes. They can inspire development and other compatible land uses at some of the heavier used locations. The planning and design of this trailhead is being considered with the Kansas City Area Transportation Authority for potential transit service in the future.

BLUE RIDGE CUTOFF TRAILHEAD PLANNING

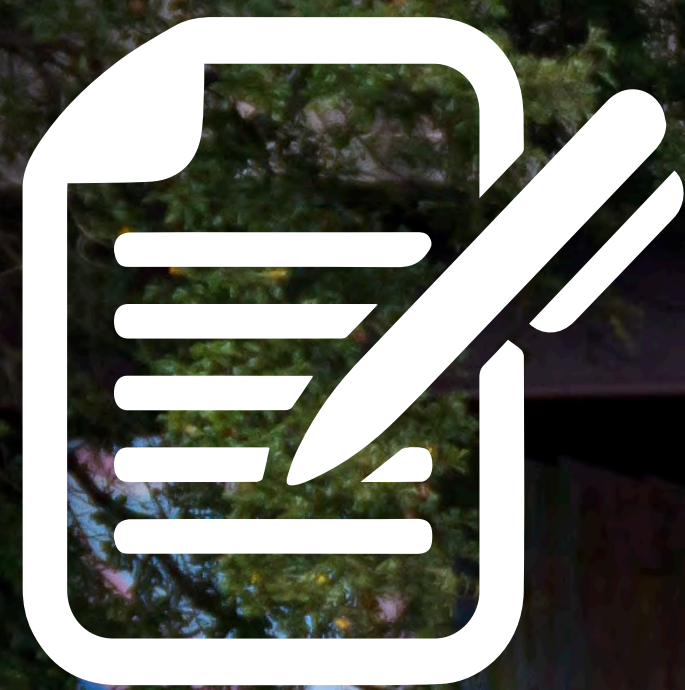


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WE WANT TO HEAR FROM YOU!

Thank you for coming tonight.

Your input is important, please share your comments by:



Filling out a comment card and leaving it here or returning it to the address shown by Friday, May 19.



Providing verbal comments to the stenographer here tonight.

NEXT STEPS

The next steps for the project include completing right of way plans for the northern segment and finalizing design plans for the southern segment of the corridor.

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